

TO: James L. App, City Manager
FROM: Doug Monn, Interim Public Works Director
SUBJECT: Adopt Plan Line for Union Road Ultimate Improvements
from Kleck to Golden Hill Road
DATE: December 20, 2005

NEEDS: For the City Council to adopt a Plan Line for the ultimate improvements for Union Road from Kleck to Golden Hill Road.

FACTS:

1. On May 18, 2004, the City Council awarded a contract to North Coast Engineering to develop a Plan Line for the ultimate improvements along Union Road from Kleck to Golden Hill Road.
2. Meetings between staff and the consultant have been on-going since May 2004, resulting in the conceptual Plan Line presented to individual members of Council for review in November.
3. Based on the results of the presentations, staff is ready to submit the Plan Line to City Council for approval.
3. North Coast Engineering will present the details of the Plan Line at the meeting. Some design items of the proposed Plan Line are:
 - Provision of a median lane
 - Pedestrian access on the north side
 - Provision of a standard "tee" intersection at Walnut
 - Provision of a bike lane on both sides
 - Ability to consider combining private driveway access
 - Consideration of a separate pedestrian crossing east of Montebello Oaks Drive
 - A roundabout at Union and Golden Hill Road

**ANALYSIS
AND**

CONCLUSION: Adoption of the Plan Line will allow staff to require future development to dedicate necessary right-of-way as part of their condition of approval for the ultimate construction of the corridor.

POLICY

REFERENCE: General Plan and its Circulation Element

FISCAL

IMPACT: There is no fiscal impact at this time in adopting the Plan Line.

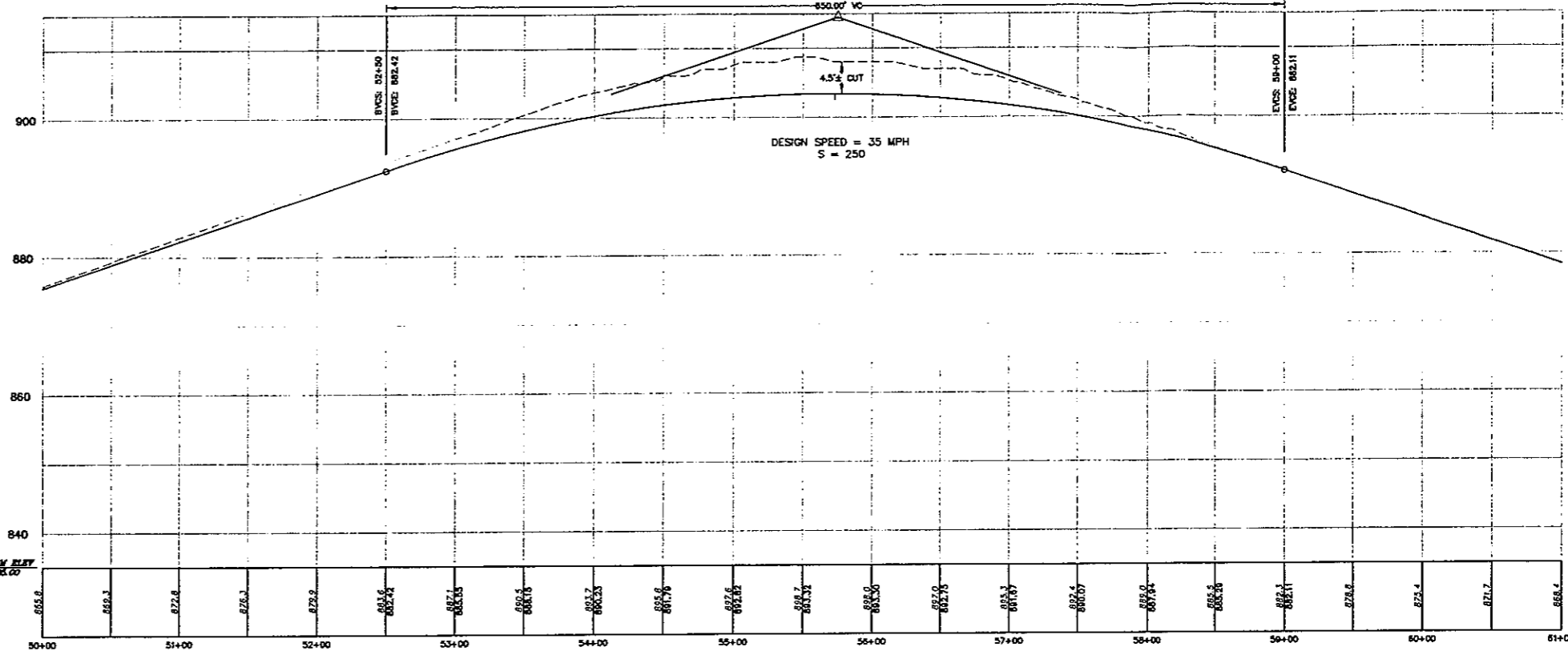
OPTIONS:

- a. Adopt the Plan Line for Union Road from Kleck to Golden Hill Road as presented.
- b. Amend, modify, or reject the above option.

Attachments

- 1) Reduced Plan Line Exhibit

HIGH POINT STA = 55+72.78
 PM STA = 55+75
 PM ELEV = 804.48
 A.D. = -13.67
 K = 47.54
 550.00' VC



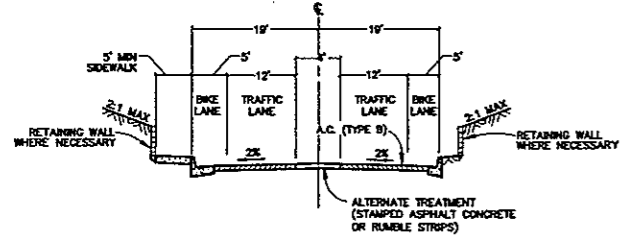
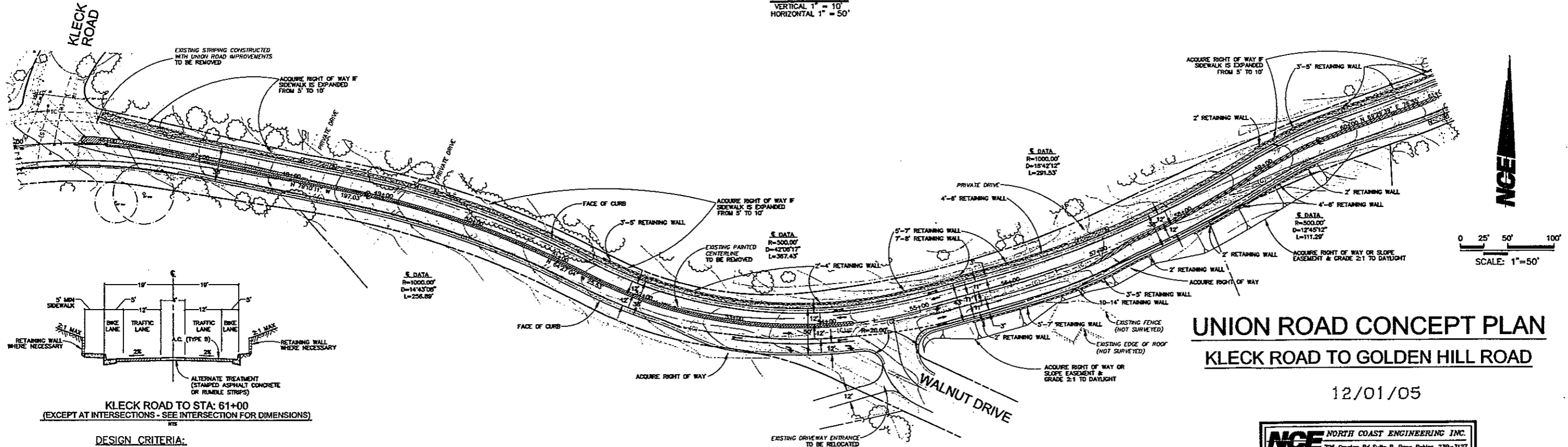
DESIGN SPEED = 35 MPH
 S = 250

LEGEND

- EXISTING TEXT
- FACE OF CURB
- RIGHT OF WAY
- 12+00 CENTERLINE STATIONING
- RETAINING WALL
- SIDEWALK
- EXISTING OAK TREE AND CRITICAL ROOT ZONE
- ~ EXISTING TREES

NOTE:
 ALL UTILITY LIDS, BOXES, FIRE HYDRANTS, UTILITY POLES, AND ETC. SHALL BE RAISED TO GRADE OR RELOCATED

PROFILE
 VERTICAL 1" = 10'
 HORIZONTAL 1" = 50'



KLECK ROAD TO STA: 61+00
 (EXCEPT AT INTERSECTIONS - SEE INTERSECTION FOR DIMENSIONS)

DESIGN CRITERIA:

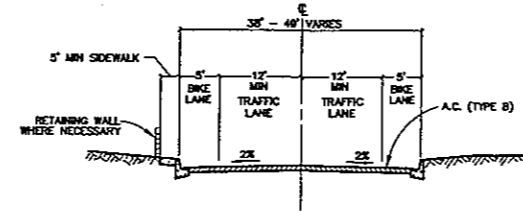
- DESIGN SPEED 35 MPH
- MINIMUM RADIUS = 425' (CALTRANS)
- EMAX = 0.06; L = 150' TRANSITION
- CLASS II BIKE PATH

UNION ROAD CONCEPT PLAN
KLECK ROAD TO GOLDEN HILL ROAD

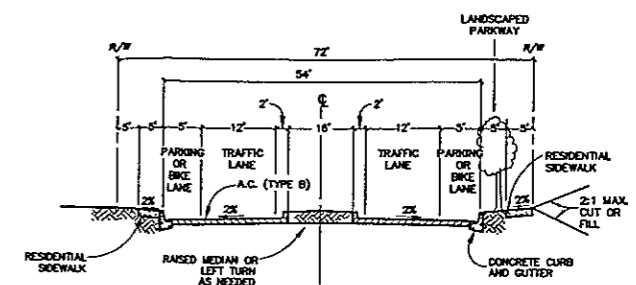
12/01/05



LEGEND	
TEXT	EXISTING TEXT
—	FACE OF CURB
- - -	RIGHT OF WAY
12+00	CENTERLINE STATIONING
—	RETAINING WALL
—	SIDEWALK
○	EXISTING OAK TREE AND CRITICAL ROOT ZONE
—	EXISTING TREES

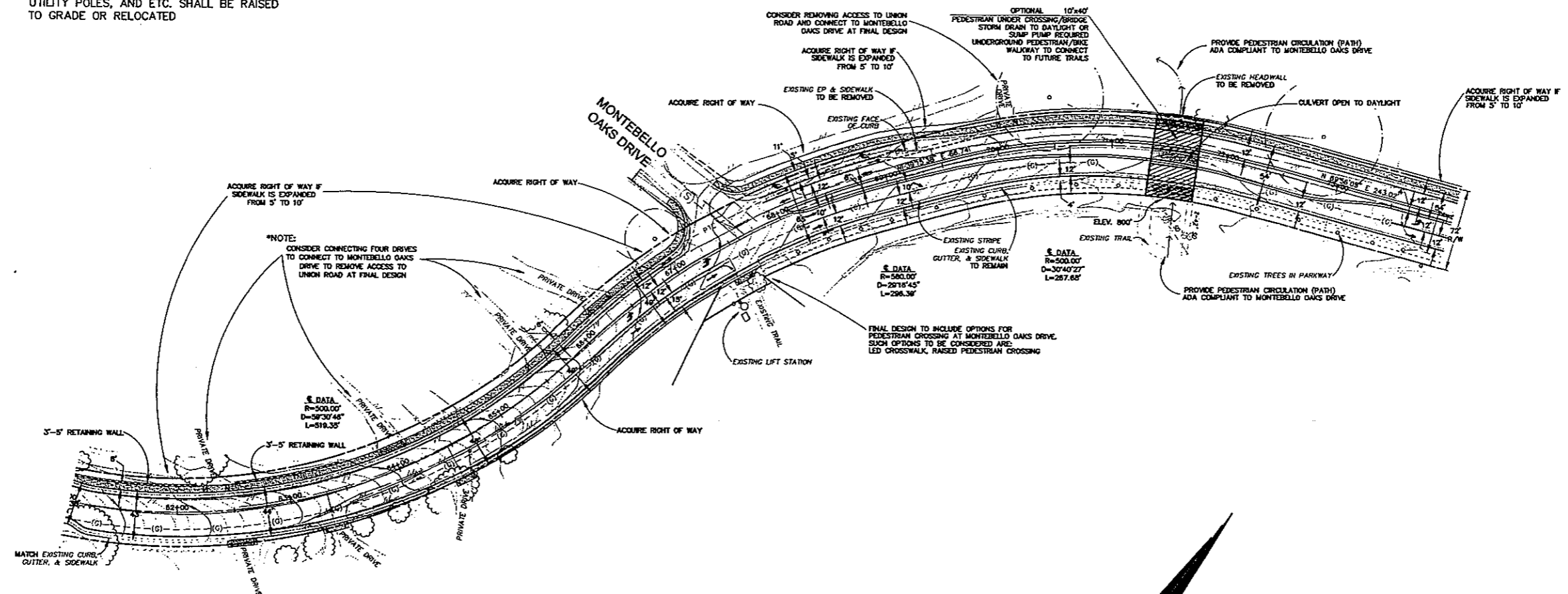


STA 61+00 TO MONTEBELLO OAKS DRIVE
 DESIGN CRITERIA: (STA 61+00 TO STA 72+00)
 - DESIGN SPEED 35 MPH
 - MINIMUM RADIUS = 425' (CALTRANS)
 - EMAX = 0.06; L = 150' TRANSITION
 - CLASS II BIKE PATH



**MONTEBELLO OAKS DRIVE TO PROSPECT AVE
 STA 72+00 TO STA 75+00**
 DESIGN CRITERIA:
 - DESIGN SPEED 45 MPH
 - MINIMUM RADIUS = 700' (CALTRANS)
 - EMAX = 0.06; L = 150' TRANSITION
 - CLASS II BIKE PATH

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UNION ROAD CONCEPT PLAN
KLECK ROAD TO GOLDEN HILL ROAD

12/01/05



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